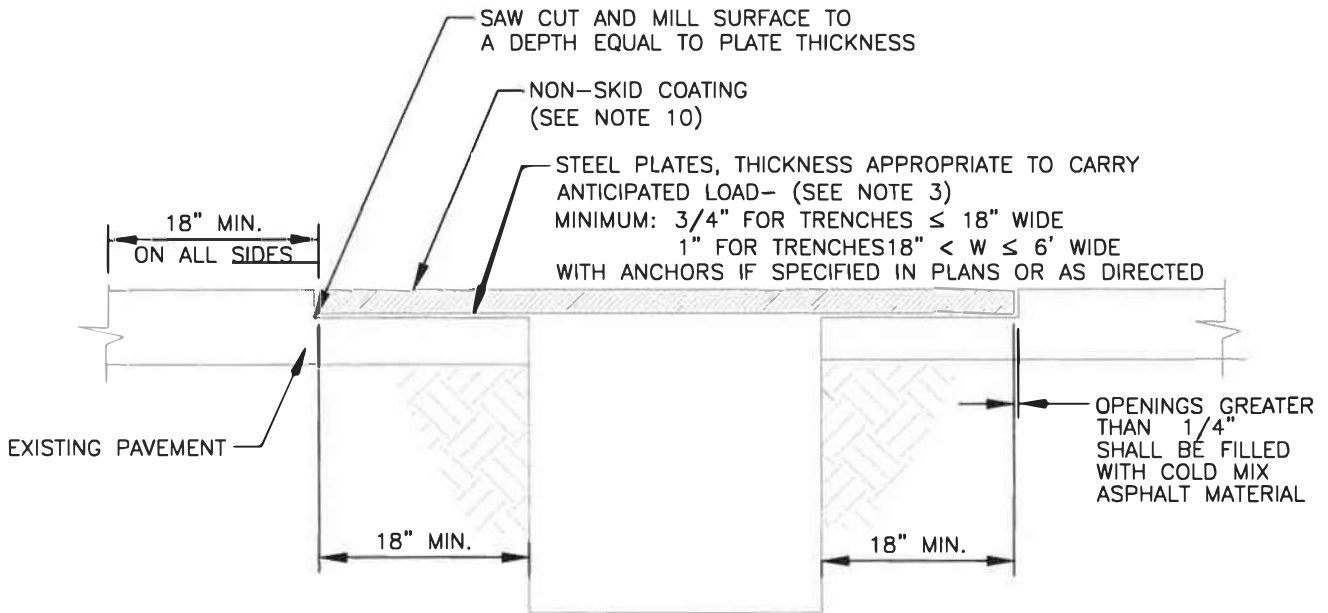



**TYPICAL CROSS SECTION**  
**CASE I - LOW TRAFFIC VOLUME/LIMITED IN-SERVICE TIME**



**TYPICAL CROSS SECTION**  
**CASE II - EXTENDED IN-SERVICE PLATING AND/OR HEAVILY-TRAFFICKED ROADS/STREETS (HMAC PAVEMENTS ONLY)**


SCALE: NTS

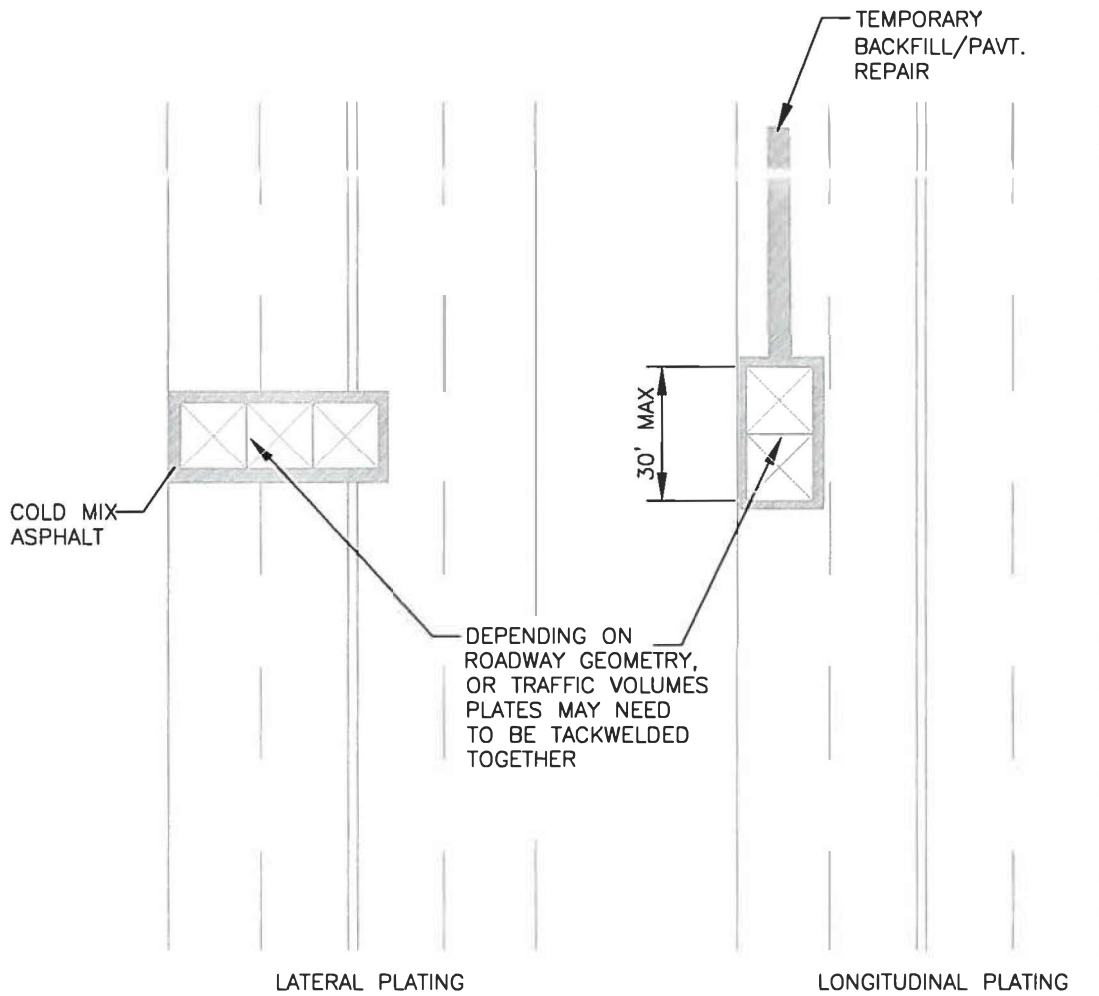
<p>RECORD SIGNED COPY ON FILE APPROVED</p>	<p><b>CITY OF ROUND ROCK</b></p>	<p>DRAWING NO: ST-24.1 SHEET 1 of 3</p>
<p>01-28-21 DATE</p>	<p><b>STEEL PLATING</b></p>	
<p>THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR THE APPROPRIATE USE OF THIS DETAIL.</p>		

**NOTES:**

1. WHERE TRAFFIC MUST CROSS TRENCHES, THE CONTRACTOR SHALL PROVIDE SUITABLE PLATES. ALL PLATES ARE TO BE INSTALLED AS DETAILED.
2. THE USE OF STEEL PLATES SHALL BE AS APPROVED BY THE ENGINEER PRIOR TO INITIATION OF CONSTRUCTION.
3. THE THICKNESS OF PLATES FOR TRENCH WIDTHS EXCEEDING 6' SHALL BE ESTABLISHED IN AN ANALYSIS COMPLETED BY A LICENSED PROFESSIONAL ENGINEER, REGISTERED IN THE STATE OF TEXAS. THE ANALYSIS SHALL BE BASED ON HS-20 TRAFFIC LOADING WITH A MAXIMUM PLATE DEFLECTION OF 1/2" WHEN EXPERIENCING SAID LOADING. FOR SITUATIONS WHERE MULTIPLE LAYERS OF PLATES (OR STACKED PLATES) ARE TO BE EMPLOYED, THE SEAMS (I.E. THE INTER-FACE BETWEEN PLATES PLACED SIDE-BY-SIDE) OF THE UPPER LAYER SHALL BE PLACED PERPENDICULAR TO THE SEAMS OF THE UNDERLYING PLATES.
4. WHEN APPROVED, THE TYPE OF PLATE INSTALLATION SHALL BE BASED ON THE ANTICIPATED LENGTH OF TIME THE PLATE WILL BE IN SERVICE:  
CASE I: A CASE I INSTALLATION SHALL APPLY FOR NO LONGER THAN A 2 WEEK PERIOD.  
CASE II: A CASE II INSTALLATION SHALL APPLY FOR LONGER THAN 2 WEEK PERIOD.
5. THE TOPSIDE OF THE STEEL PLATE SHALL BE FLAT AND FREE OF ANY CLIPS, CHAINS, ATTACHMENTS, WELDMENTS OR SURFACE IRREGULARITIES.
6. PLATES WITH A PERMANENT DISPLACEMENT (I.E. DISPLACEMENT ANYWHERE ON THE SURFACE OF THE PLATE WITH RESPECT TO A PLANE FORMED BY THE OUTSIDE EDGES) THAT EXCEEDS 1/2" SHALL NOT BE USED FOR PLATING PURPOSES. PLATES THAT DEVELOP A PERMANENT DISPLACEMENT EXCEEDING 1/2" DURING SERVICE SHALL BE REMOVED AND REPLACED.
7. THE PLATES SHALL BE PROVIDED WITH APPROPRIATE NUMBER OF KEYHOLE SLOTS OR CIRCULAR HOLES FOR HANDLING, LIFTING, INSTALLATION AND REMOVAL PURPOSES.
8. THE CONTRACTOR SHOULD AVOID USING A LONG SERIES OF PLATES THAT RUN PARALLEL TO VEHICULAR TRAFFIC WHEELS PATHS.
9. ADDITIONAL METHODS OF SECURING PLATES MAY BE REQUIRED DEPENDING ON FIELD CONDITIONS.
10. FOR PLATES 6' OR GREATER IN DIRECTION OF TRAFFIC, A NON-SKID COATING SHOULD BE APPLIED TO THE ENTIRE SURFACE AREA OF ALL PLATES, AS WELL AS ADJACENT AREAS. THE NON-SKID COATING SHALL BE TCA (TEXTURED COATING OF AMERICA, INC.) STRATA-GRIP DECK COATING SYSTEM; SLIPFIX, INC. SPS (SLIP PROTECTION SURFACE) OR AN EQUIVALENT PRODUCT APPROVED BY THE ENGINEER.
11. FURNISH FOR ENGINEER'S APPROVAL THE PLATE LAYOUT, CONSTRUCTION METHODS, AND ANTICIPATED DURATION PRIOR TO CONSTRUCTION.

**SCALE: NTS**


<b>RECORD SIGNED COPY ON FILE</b> <hr/> <b>APPROVED</b>	<b>CITY OF ROUND ROCK</b>	<b>DRAWING NO:</b> <b>ST-24.2</b> <b>SHEET 2 of 3</b>
<b>01-28-21</b> <hr/> <b>DATE</b>	<b>STEEL PLATING</b>	
<b>THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR THE APPROPRIATE USE OF THIS DETAIL.</b>		



**NOTE:**

REFER TO TRAFFIC CONTROL PLAN FOR PHASING OF EXCAVATIONS

SCALE: NTS

<p>RECORD SIGNED COPY ON FILE</p> <hr/> <p>APPROVED</p>	<p><b>CITY OF ROUND ROCK</b></p>	<p>DRAWING NO: ST-24.3 SHEET 3 of 3</p>
<p>01-28-21</p> <hr/> <p>DATE</p>		
<p>THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR THE APPROPRIATE USE OF THIS DETAIL.</p>	<p><b>STEEL PLATING</b></p>	